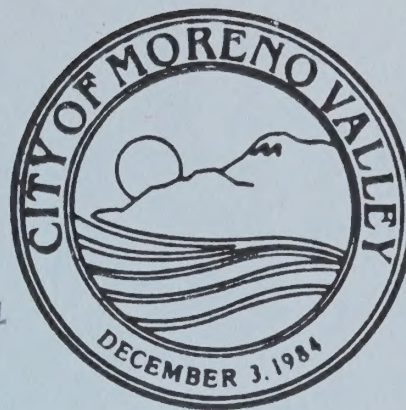


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INTERIM CITY GUIDELINES



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INTERIM CITY GUIDELINES



TABLE OF CONTENTS

SECTION I - GENERAL PROVISIONS

| | |
|--------------------------------------|---|
| INTENT | 2 |
| STATEMENT OF PURPOSE | 2 |
| DEVELOPMENT STANDARDS | 2 |
| DEVIATIONS FROM GUIDELINES | 3 |

SECTION II - DESIGN GUIDELINES

| | |
|-----------------------------|---|
| INTENT | 6 |
| GENERAL PROVISION | 6 |

SITE DESIGN

GUIDELINES:

| | |
|-----------------------|----|
| GENERAL | 8 |
| RESIDENTIAL | 10 |
| COMMERCIAL | 13 |
| INDUSTRIAL | 15 |

GRADING

GUIDELINES:

| | |
|---------------------|----|
| GENERAL | 15 |
| STANDARDS | 16 |

HILLSIDE DEVELOPMENT

| | |
|-------------------|----|
| INTENT: | 18 |
|-------------------|----|

GUIDELINES:

| | |
|-------------------|----|
| GENERAL | 20 |
|-------------------|----|

STANDARDS

| | |
|------------------------------------|----|
| NATURAL OPEN SPACE AREAS | 22 |
| LANDSCAPING | 27 |
| GRADING | 28 |
| ROADWAYS/CIRCULATION | 29 |
| FIRE PROTECTION | 30 |

ARCHITECTURAL DESIGN

GUIDELINES:

| | |
|-------------------|----|
| GENERAL | 30 |
|-------------------|----|

ACCESS/CIRCULATION/PARKING

GUIDELINES:

| | |
|-------------------|----|
| GENERAL | 33 |
|-------------------|----|



TABLE OF CONTENTS

| | |
|--------------------------------|----|
| STANDARDS: | |
| ACCESS | |
| CIRCULATION | |
| PARKING | 35 |
| PUBLIC FACILITIES AND SERVICES | |

| | |
|-------------------|----|
| GUIDELINES: | |
| GENERAL | 37 |

| | |
|----------------------------------|----|
| STANDARDS: | |
| UTILITIES | 40 |
| FLOOD CONTROL | 40 |
| POLICE/FIRE PROTECTION | 40 |

LANDSCAPING AND OPEN SPACE

| | |
|------------------|----|
| INTENT | 41 |
|------------------|----|

| | |
|-------------------|----|
| GUIDELINES: | |
| GENERAL | 41 |

| | |
|---------------------------------|----|
| STANDARDS: | |
| RESIDENTIAL | 44 |
| COMMERCIAL/INDUSTRIAL | 44 |
| PLANT MATERIAL | 46 |
| OPEN SPACE | 46 |

SIGNAGE

| | |
|-------------------|----|
| GUIDELINES: | |
| GENERAL | 47 |

LIGHTING

| | |
|-------------------|----|
| GUIDELINES: | |
| GENERAL | 50 |

SCREENING

| | |
|-------------------|----|
| GUIDELINES: | |
| GENERAL | 51 |

| | |
|-----------------------------|----|
| GLOSSARY OF TERMS | 54 |
|-----------------------------|----|

| | |
|--|----|
| GROUPS/INDIVIDUALS CONSULTED | 56 |
|--|----|

TABLE OF CONTENTS

TABLE OF CONTENTS

CHAPTER I. INTRODUCTION

CHAPTER II. THEORY

CHAPTER III. EXPERIMENT

CHAPTER IV. RESULTS

CHAPTER V. DISCUSSION

CHAPTER VI. CONCLUSION

CHAPTER VII. REFERENCES

CHAPTER VIII. APPENDIX

CHAPTER IX. INDEX

CHAPTER X. GLOSSARY

CHAPTER XI. BIBLIOGRAPHY

CHAPTER XII. LIST OF FIGURES

CHAPTER XIII. LIST OF TABLES

CHAPTER XIV. SUMMARY

CHAPTER XV. ACKNOWLEDGMENTS

CHAPTER XVI. CURRICULUM VITAE

CHAPTER XVII. RESUME

CHAPTER XVIII. BIOGRAPHY

CHAPTER XIX. AUTOBIOGRAPHY

CHAPTER XX. MEMOIRS

CHAPTER XXI. JOURNAL

CHAPTER XXII. DIARY

CHAPTER XXIII. RECORD

CHAPTER XXIV. LOG

CHAPTER XXV. ACCOUNT

CHAPTER XXVI. STATEMENT

CHAPTER XXVII. REPORT

CHAPTER XXVIII. SUMMARY

CHAPTER XXIX. CONCLUSION

CHAPTER XXX. REFERENCES

CHAPTER XXXI. APPENDIX

CHAPTER XXXII. INDEX

CHAPTER XXXIII. GLOSSARY

CHAPTER XXXIV. BIBLIOGRAPHY

CHAPTER XXXV. LIST OF FIGURES

CHAPTER XXXVI. LIST OF TABLES

CHAPTER XXXVII. SUMMARY

CHAPTER XXXVIII. ACKNOWLEDGMENTS

CHAPTER XXXIX. CURRICULUM VITAE

CHAPTER XL. RESUME

CHAPTER XLI. BIOGRAPHY

CHAPTER XLII. AUTOBIOGRAPHY

CHAPTER XLIII. MEMOIRS

CHAPTER XLIV. JOURNAL

CHAPTER XLV. DIARY

CHAPTER XLVI. RECORD

CHAPTER XLVII. LOG

CHAPTER XLVIII. ACCOUNT

CHAPTER XLIX. STATEMENT

CHAPTER L. REPORT



DEVIATIONS FROM GUIDELINES

SECTION I - GENERAL PROVISIONS



INTENT

This Chapter contains the Interim City Guidelines which regulate and guide future development until final City goals, objectives, policies, and guidelines are adopted as part of the General Plan. Issues addressed in this division include: uses, design standards, and performance standards. These standards represent the minimum design and development criteria to be met by any new development project within the City of Moreno Valley. These guidelines are intended to supplement adopted ordinances and regulations controlling new development and are not intended to replace existing said regulation or ordinance.

STATEMENT OF PURPOSE

The intent of the Interim City Guidelines for the City of Moreno Valley is to insure that the development of the City, prior to the completion of the General Plan, will be of high quality, and will not preclude options now open to the General Plan program. The City has substantial quantities of developable land which is highly visible to the public. Therefore, to provide a pleasing environment for existing and future residents and to attract major businesses, a consistency in good design is desired by the City Council.

DEVELOPMENT STANDARDS

All land use applications shall incorporate the development standards contained within this division. Provided, however, that deviation from standards may be requested to be reviewed by the Planning Commission by either the applicant or City of Moreno Valley staff or advisory bodies.

INTERIM CITY GUIDELINES



DEVIATIONS FROM GUIDELINES

Deviation from design standards should not be permitted unless the Planning Commission can find that the proposed deviation is consistent with the existing and proposed General Plans and will further the objectives of enhancing the health, safety and welfare within the City of Moreno Valley.

ADDITIONAL INFORMATION

There are several documents besides this one, of which the developer should become aware. These guidelines clarify or make reference to portions of these documents but are not meant to supercede them.

The developer should be acquainted with:

City of Moreno Valley Zoning Code
City of Moreno Valley Redevelopment Plan
City of Moreno Valley Municipal Code
Sunnymead Boulevard Specific Plan
City Ordinance No. 45
U.B.C. 70 as modified by the City of Moreno Valley

These documents are available from the City of Moreno Valley Planning Department at:

City of Moreno Valley
Administrative Center
12810 Heacock Street
P.O. Box 1440
Moreno Valley, CA 92388
Bus. Hrs. 8:00 - 5:00 p.m.



SECTION II – DESIGN GUIDELINES

INTERIM CITY GUIDELINES



INTENT

The purpose of design standards is to insure that the built environment within the City of Moreno Valley is compatible with the nature and use of each land use and each land use zone. The standards and guidelines are intended to result in a well-designed environment which is safe and pleasing to work and live within.

It is the intent of the City of Moreno Valley to have compatible architecture and enhanced design quality throughout the City. It is also the intent of the City to recognize existing developed areas and the need for preservation, improvement, or replacement of design and land uses.

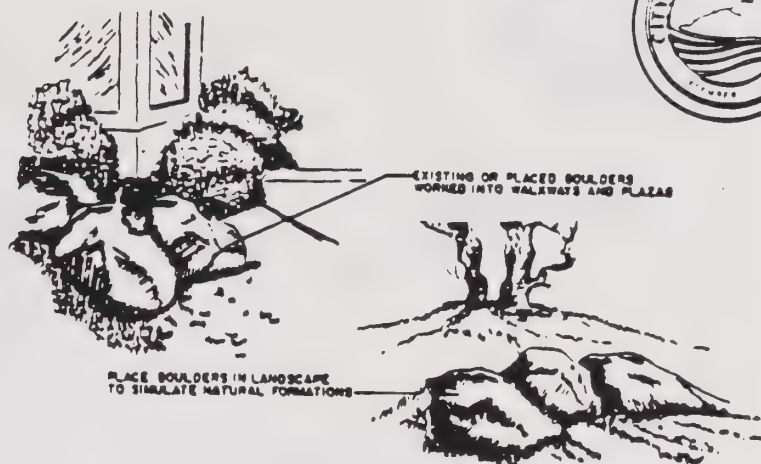
As a consequence, some of the following design standards are general and, through the design review process, are intended to encourage the individual creativity of project designers, as well as to provide for the needs of the individual owner and user.

These guidelines apply to all projects submitted and in the process of project review as determined by the Director of Developmental Services.

GENERAL PROVISIONS

1. New development or the alteration or enlargement of existing development should be viewed not only as free-standing objects, but also as part of a street, cluster, or neighborhood within the entire community. Parcel or lot configurations should respect the natural environment; existing development; topography; views; and general vehicular, pedestrian, bicycle, and equestrian circulation.

INTERIM CITY GUIDELINES



2. Natural features should be used to an advantage as design elements; such as, mature vegetation, landforms, drainage courses, rock outcroppings and views. Conversely, undesirable site features can be minimized through proper site planning and building orientation (see above example).
3. All projects are encouraged to establish a recognizable design theme, which is compatible with surrounding planned or existing land developments. This design theme should be based upon prominent design features existing in the immediate area (i.e., trees, landforms, historic landmarks). Subtle variations which provide visual interest, but do not create abrupt changes or cause discord in the overall character of the neighborhood are encouraged. In addition, transitions between different projects, including provision of buffer areas, landscaping, and other similar treatments are to be provided as necessary. It is not, however, intended that one style of architecture should be dominant, but that individual structures create and enhance a high quality and harmonious appearance.



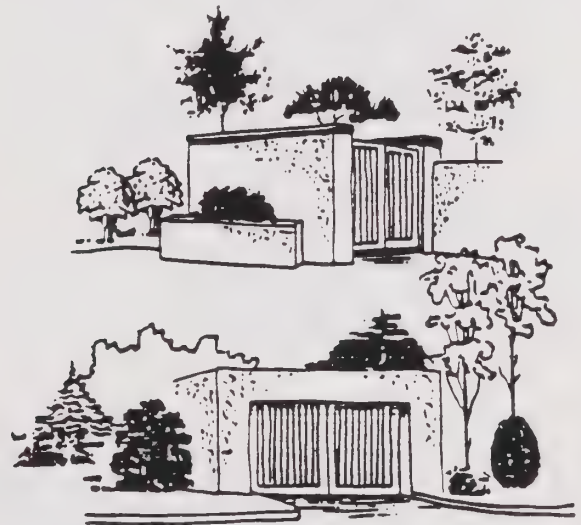
4. Where a common area or facility is proposed within a residential, commercial, or industrial project, a property owners association or other approved mechanism shall be established prior to the issuance of any building permits within the first phase of the project. The property owners association or other approved mechanism shall be responsible for private roads, parking areas, signing, landscaped areas, irrigation, common areas, and other responsibilities as necessary.

SITE DESIGN

1. New development should be designed to create pleasing transitions to surrounding development. The bulk of new structures should relate to the prevailing or planned scale of adjacent development. The setback from streets and adjacent properties should relate to the scale of the proposed structure. Larger structures require more setback area for a balance of scale and compatibility with adjacent uses. In addition, tall buildings should be made less imposing by physically stepping them back from the street level.
2. Gateways are visual elements which create a sense of entry, and should be included in all development projects. Gateway treatments can range in scale, and may identify an entrance to the City, subcommunity, development project or a single building. Gateways should include enriched paving, raised medians, signage, and other features as appropriate.



3. New developments should be designed so as to respect the views of existing developments, whenever possible. View corridors which are oriented toward an existing or proposed amenity such as a park, open space or natural feature (water, topography, vegetation, etc.) should be designed through large developments. (see above).



4. Trash enclosures, loading areas, mechanical equipment, and outdoor storage areas shall be screened as appropriate to minimize public views of such otherwise unaesthetic features. Enclosures shall be designed with materials which are compatible with the project architecture and do not detract from the overall design theme. (see above).

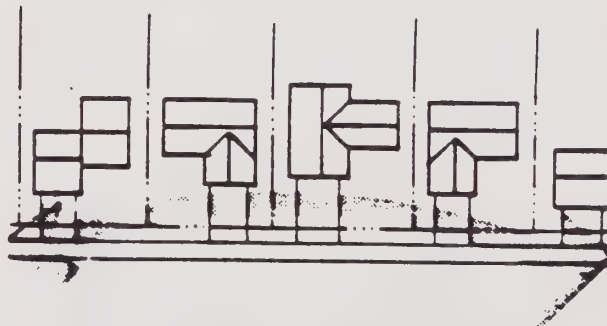
INTERIM CITY GUIDELINES



5. Except in hillside situations where alternatives are not available, new flag lots shall not be permitted.
6. Natural drainage courses should be retained in their natural state where feasible.

Residential Guidelines

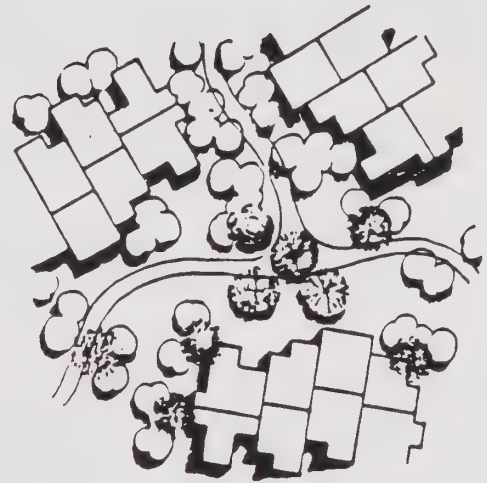
1. Lots should not be sited with rear yards facing the entrance to a neighborhood. Where this situation cannot be avoided, a combination of wall and bermed landscaping treatment shall be provided as approved by the Planning Department and the Architectural Review Committee.
2. Within multiple family projects, private entry areas to individual units should be provided.



3. Residential units should be provided with a minimum five feet variation in their front and side yard setbacks along the streetscape in addition to required setbacks specified in the zoning ordinance, if possible (see above).



4. In higher density units with tuck-under parking and/or opposing garages, individual units shall be turned and oriented in a variety of ways to avoid the monotony of garage door corridors. Alternatively, rear parking garages should be staggered and appropriately landscaped.
5. Location of individual unit entry and garages should vary with side and front entry garages encouraged.

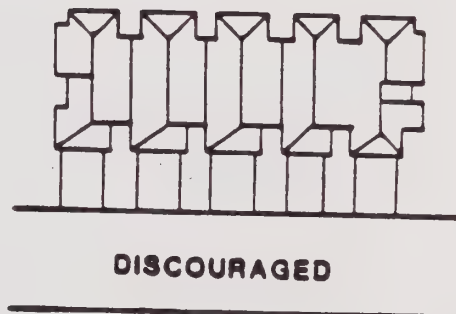
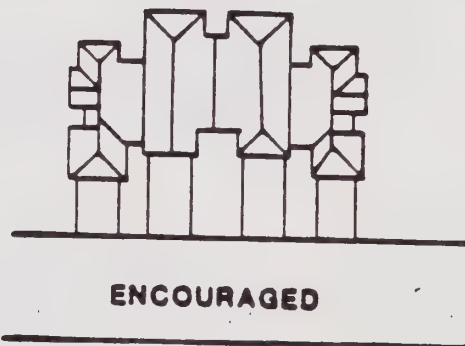


6. All multiple family and small lot single family residential projects shall include open space areas for the enjoyment of residents. This may be accomplished by clustering of units around courts or open greens, active or passive recreation areas, or winding greenbelts through a development (see above).
7. Active recreation areas should be provided within all multiple family areas. These may consist simply of landscaped open space, although provisions for recreation facilities such as pools, tennis courts, exercise rooms, etc. are highly recommended.

INTERIM CITY GUIDELINES



8. Each multiple family unit should have 100 square feet of private open space which may consist of a fenced yard area, balcony, or patio. The minimum dimension of such open space should be eight (8) feet (5 feet for a balcony).



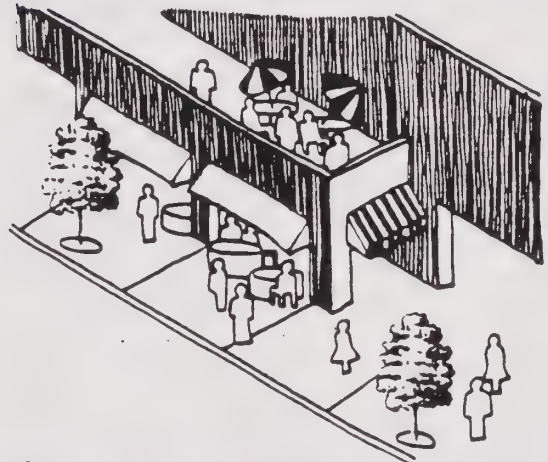
9. Residential projects should maximize the feeling of openness by curving streets, varying front and side yard setbacks, and, if possible, orienting road axes to open space areas and views (see above).
10. Small lot single family subdivisions should utilize innovative design techniques to provide more usable and private exterior area. In addition, small lot subdivisions should maintain a minimum lot width of 50 feet; 60-foot lot widths (measured at building setback line) are preferable.
11. Residential mechanical equipment including but not limited to air conditioning units, cable T.V. equipment, irrigation back-flow devices, shall be screened from public rights-of-way or ground mounted and screened with landscaping where feasible.

INTERIM CITY GUIDELINES



Commercial Guidelines

1. Commercial buildings and projects should be designed so as to have a central place or main focus or feature (see "gateways" under Site Design section).



CREATE A PEDESTRIAN ENVIRONMENT WITH VARIED AND INTERESTING SHOPS AND RESTAURANTS.

2. New development should be oriented toward pedestrians. Buildings should be sited so as to create a human-scale environment, including identifiable pedestrian spaces. Uses within pedestrian spaces should contribute to a varied and lively streetscape (see above).
3. Buildings facing pedestrian ways and plazas should incorporate visual design features that provide visual interest at the street level.
4. Building placement within office developments should occur at or near the setback line in order to project a desirable architectural image contiguous to the street, and to remove parking lots to the extent possible from the streetscape.



VARIED BUILDING SETBACKS CREATE PLAZA-LIKE AREAS.

5. Building setbacks within projects and along major pedestrian should be varied to create plaza-like areas which attract pedestrians whenever possible (see above).
6. Loading areas shall be provided, and should be oriented away from street side elevations whenever possible, or shall be screened from public view with a combination of walls and landscaping.
7. Large structures should incorporate setbacks and variations in massing of building bulk along major streets whenever possible to provide variety and visual interest to the streetscape. Continuous, blank building elevations shall not be plotted parallel to public streets.
8. Service stations mini markets and other automobile related uses proposed at corner locations should orient away from the street frontage. Rear building elevations are to be oriented to the street intersection, and shall have architectural details consistent with the overall project design and shall be landscaped with a combination of landscaping and berming. Access to service bays will be from the interior of the service station site.

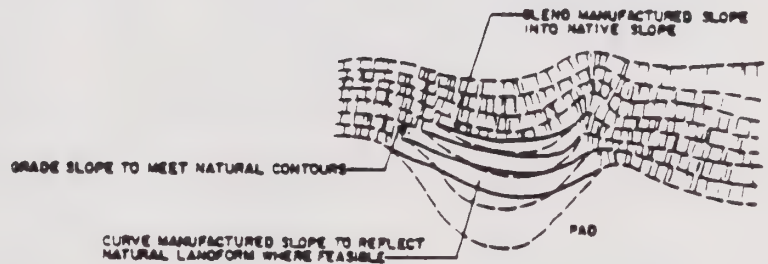
INTERIM CITY GUIDELINES

Industrial Guidelines



1. Solid walls and landscaping should be used to screen loading, service, and trash storage areas from public view. In addition, solid walls and landscaping should be used to screen ground-mounted (such as transformers) equipment located away from buildings (see above example).
2. Truck docks and trash storage areas must be closed off by roll-down or another type of door. These doors should be arranged in an organized manner, integrated within the overall design of the industrial project, and oriented away from public view.

GENERAL GRADING GUIDELINES



1. Development should relate to the natural surroundings and minimize grading by following the natural contours as much as possible. Graded slopes should be rounded and contoured so as to blend with the adjacent natural terrain (see above).

INTERIM CITY GUIDELINES



2. No grading other than that exempted by City Ordinance No. 45 and approved stock pile/borrow plans with the appropriate permits, shall be permitted prior to the recordation of a final tract map or in the case of single lot developments, prior to plan approval with the appropriate bonding and permits.
3. Concurrent with the submittal of a grading plan, soils and geology reports, erosion control plans, hydrology/hydraulics calculations, dust control plans, and landscaping plans shall be submitted to the Engineering Department and approved by the appropriate departments prior to the approval of the grading plan and submittal of an application for the grading permit.

Grading Standards

1. Erosion Control:
 - a. Erosion control plans shall be prepared by a registered civil engineer and shall cover all areas impacted by the proposed grading. The plan shall address methods of control (i.e. desilting basins, check dams, sandbagging, etc.), interim storm drain construction and the name of the responsible person (with phone number) for after hour emergencies.
 - b. Erosion control measures shall be in place prior to any forecasted rain and throughout the rainy season. The measures shall be placed as soon as possible during the grading operation and remain in operation until improvement construction has begun within the controlled area.



c. Slope protection shall be provided as specified per City Ordinance No. 45. All fill slopes over 3' and cut slopes over 5' high shall be landscaped. Landscaping shall be installed and certified prior to the issuance of the certificate of occupancy.

2. Setbacks (for tops and toes of graded slopes)

a. Setbacks adjacent to existing and proposed property lines shall be as specified per U.B.C. 70 as modified by City Ordinance 45.

b. All slopes shall be graded to place property lines (existing and proposed) at the top of the slope. If this is not possible, the toe of the slope shall be setback a minimum of 5' from the existing property line with drainage control installed at the toe, within a 10' minimum width drainage easement.

c. Tops and toes of slopes adjacent to street right-of-ways shall be setback a minimum of 2 feet for slopes less than 10 feet in height and 3 feet for slopes over 10 feet in height.

3. Slope Standards

a. Cut and fill slopes shall not be constructed steeper than 2:1.

b. All slopes greater than 25 feet in height shall be provided with benching, trenching, and downdrains as specified by U.B.C. 70. Care shall be taken in the placement of these structures to hide them from public view by design and/or landscaping.



c. Slopes constructed adjacent to roadways shall be designed to enhance their visual impacts, by the use of variable slope ratios, meandering tops and toes, integrated landscaping with right-of-way areas.

4. Slope Landforming

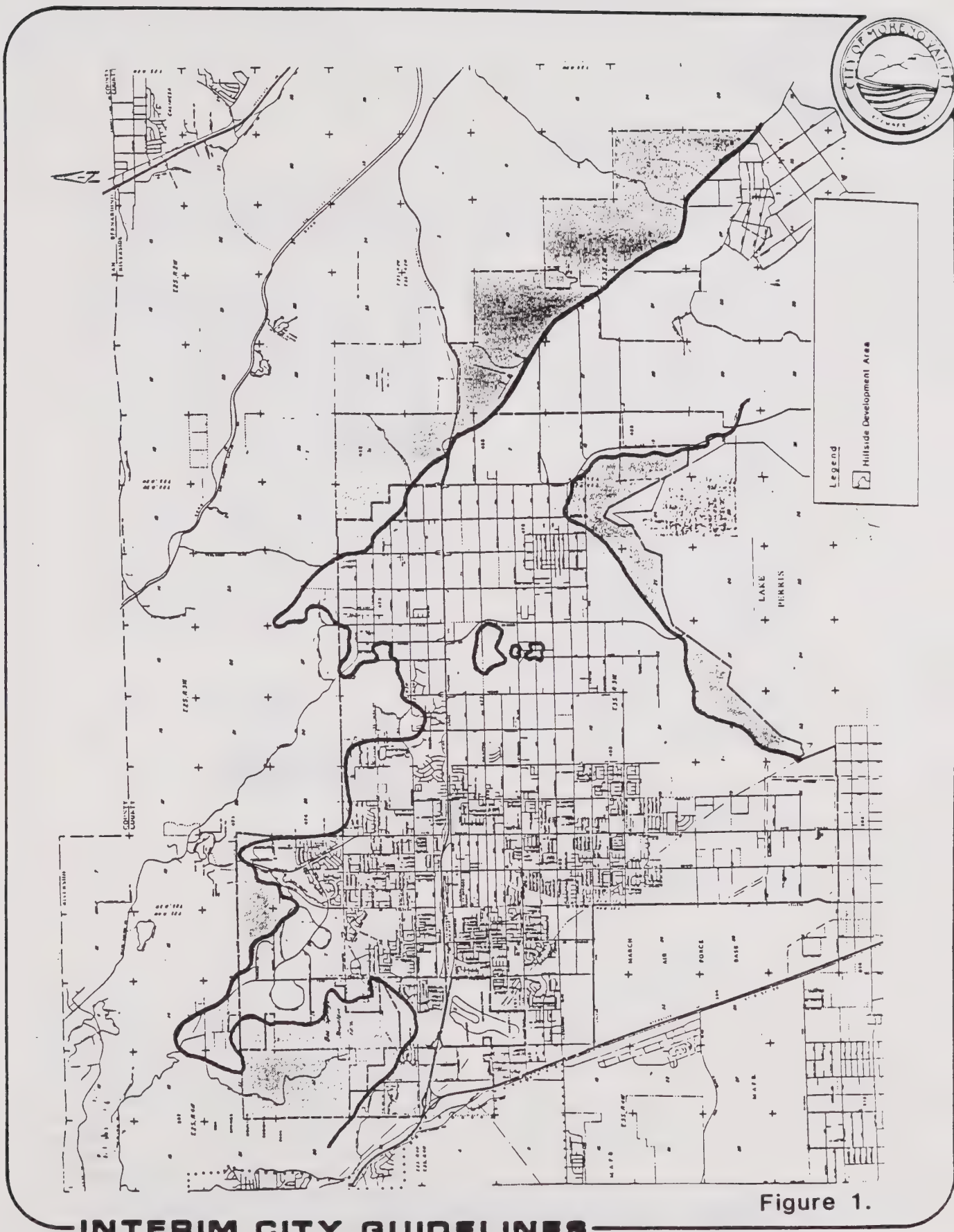
a. Landforming shall be accomplished by the use of variable slope ratios, undulating tops and toes, hiding of terraces and downdrains, and constantly varying surface features and landscaping.

b. All slopes greater than 100' in length or 10' and greater in height located in areas as specified above shall be landformed.

c. Landforming shall be applied to all slopes constructed adjacent to City streets, entry areas and public view areas. Rear and side yard slopes between adjacent lots within the same project need not be landformed.

HILLSIDE DEVELOPMENT INTENT

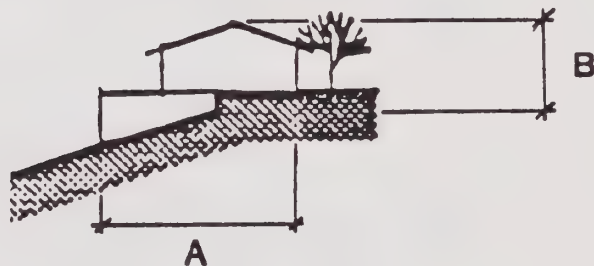
Hillside development can offer opportunities for spectacular views from building sites around the perimeter of the valley. It is important, however, to ensure that all are protected when designing hillside building sites. The guidelines in this section apply to areas outlined in Figure 1.





General Guidelines

1. All General Grading guidelines and standards shall apply to hillside development unless herein modified.
2. Where the average slope exceeds 25%, a geology report assessing rock slide potential shall be submitted to the Planning Department at the time of submittal of the project to the City. The report shall be approved by the City Engineer prior to Planning Commission approval.
3. Prior to the submittal of a project application where the average slope exceeds 10%, the applicant shall meet with an Application Team at a preapplication conference to discuss City concerns and requirements regarding the proposed hillside development.



A. Building Pad

B. Height

4. Building heights in excess of 30 feet measured vertically above the average finished ground level of the building pad, being that area under the building, to the highest point of the coping of a flat roof or to the deckline of a mansard roof or the average height of the highest gable of a pitched or hipped roof shall require special review by the Director of Developmental Services. (see above).

INTERIM CITY GUIDELINES



5. All lighting shall be low scale and low intensity and directed downward and away from the view of others.
6. Dwelling units and structures shall be sited in a manner that will:
 - a. Retain outward views from each unit;
 - b. Preserve or enhance vistas, particularly those seen from public places;
 - c. Preserve visually significant rock outcrops, natural hydrology, native plant materials, and areas of visual historic significance.
7. In areas adjacent to a ridgeline or in moderate slope areas, dwelling units and structures shall be sited to:
 - a. Use the natural ridge-line as a backdrop for structures;
 - b. Use landscape plant materials as a backdrop; and
 - c. Use structure to maximize concealment of cut slope.
 - d. Retain major natural skyline profiles.
8. Encourage larger lot sizes for areas within the City as identified in Figure 1. of these guidelines.

INTERIM CITY GUIDELINES



Hillside Development Standards

1. Natural Open Space Area

- a. All hillside development proposals shall include a portion of the site which is to be retained in natural area open space.
- b. The natural open space area may be comprised of two types: i.e., undeveloped natural areas (that which has been left in its natural state) and restored natural areas (that which has been disrupted due to construction activities).
- c. The following shall be used as guidelines in location of natural areas:
 1. Contiguous areas along drainage channels;
 2. Ten foot minimum widths adjacent to roadway improvements;
 3. Areas contiguous with other natural areas of adjacent properties;
 4. Areas containing rock outcroppings;
 5. Areas containing soils with high permeability;
 6. Significant stands or clusters of native vegetation;
 7. Known or discovered archaeological or paleontological sites.

INTERIM CITY GUIDELINES



d. The following shall be used as guidelines in locating restored natural areas:

1. In required setbacks;
2. Adjacent to common recreation facilities;
3. Along roadway improvements or over utility easements
4. Fuel modification zones as required by the Fire Department may be credited toward restored natural areas.

e. The percent of the total site required to be retained in natural open space shall be based on the average slope of the land surface. The following natural open space area requirements shall apply:

| <u>Slope Unit</u> | <u>Average Percent Slope</u> | <u>Minimum % of Slope Unit To Remain As Natural Area Open Space</u> |
|-------------------|----------------------------------|---|
| I | 10.0 - 15 | 35 |
| II | 15.1 - 25 | 50 |
| III | 25.1+ | 60 |

The natural open space requirement for each slope category shall be fulfilled within the area of that land slope.



f. The natural open space area requirement may be comprised of two types: i.e., undeveloped natural areas and restored natural areas. The schedule with which these types may apply to the natural open space area requirement is (see also Fig. 2.):

1. Undeveloped natural areas shall constitute a minimum of 75% of the required open space.
2. Restored natural areas may meet a maximum of 25% of the required open space.
3. No grading, excavation or construction except Planning Department approved landscaping with native organic or inorganic materials shall be allowed within areas designated as "natural areas".
4. Where on-site construction has inadvertently scarred designated natural areas, the applicant shall submit a design and program to the Planning Director for the restoration of the affected sites. If scarred areas exceed ten percent (10%) of the total required for the

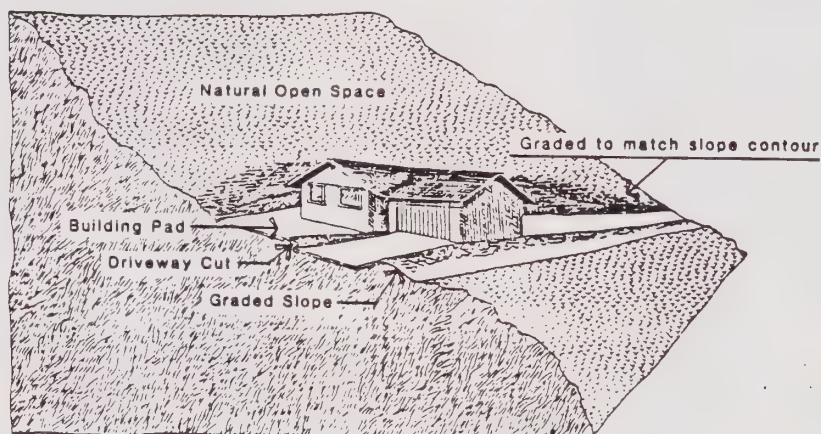


slope unit on the property, the applicant shall dedicate additional natural area to being the total natural area back to within ninety (90%) percent of the base requirement.

5. Restored natural areas shall be graded to blend in with the natural terrain and landscaped with rock and plant materials which are native to the local area.
6. All natural areas and restored natural areas shall be clearly shown on all proposed plans and building permit site plans unless otherwise approved by the City Council. A landscaping program for restored natural areas shall be approved by the Planning Department prior to issuance of building permits.
7. Existing road scars, excavations, road cuts and dump sites which are restored may count toward natural areas at twice their area.



Natural Open Space Area Requirement EXAMPLE



| | |
|-----------------------------------|--------------|
| Lot Size | 87120 |
| 19% Average Slope | |
| 50% Open Space Area Requirement | x 50% |
| Total Natural Open Space Required | <u>43560</u> |

| | | |
|------|---|--------------|
| 75% | Natural Open Space Area | 32670 |
| 23% | Restored Natural Open Space (Graded Area) | 10019 |
| 2% | Restored to Natural Open Space (Driveway Cut) | 871 |
| 100% | Total Open Space Area Requirement | <u>43560</u> |

Figure 2.



2. Landscaping

- a. Existing vegetation shall be retained in any part of the total development which is not designated for grading or construction.
- b. Existing vegetation which cannot be preserved in place but is suitable for transplanting shall be relocated elsewhere on the site.
- c. Unimproved disturbed areas shall be landscaped within 180 days following issuance of a grading permit and/or within 30 days prior to the issuance of a Certificate of Occupancy.
- d. All required plans for landscaping shall be approved prior to issuance of building permit, and all installations shall be in accordance with the approved landscape plan.
- e. All landscaping within public easements or areas to be dedicated to the City shall be low maintenance, low water consumptive vegetation.
- f. An open space network shall be designed to provide contiguous linkages throughout the development parcel, and between adjacent parcels where major public trail systems or vista corridors are designated.



3. Hillside Grading

- a. Man-made slopes shall conform to patterns of the natural terrain.
- b. All cut and fill slopes shall be landscaped and shall be stabilized to control erosion.
- c. Any cut or fill slope which is to be revegetated shall not be steeper than a 2:1 slope.
- f. All excess excavated material shall be removed or otherwise placed so as to become an integral part of the site development.
- g. During construction, measures such as sediment traps or terracing shall be used around all graded areas to minimize erosion (see general grading standards under Site Design section).
- h. Whenever archaeological or paleontological sites are discovered during excavation, the Planning Department shall be notified immediately and mutually agreeable precautions taken to preserve the artifacts.
- i. Natural features such as trees with four-inch and larger diameters and significant rock outcrops shall be protected to the greatest extent feasible in the siting of individual



lots and building pads. These features shall be shown on the grading plan with appropriate protection and relocation notes. Where tree with four inch or greater diameter are to be removed, they shall be replaced with minimum 15-gallon trees of the same species, or as approved by the Planning Department, at a ratio of three new trees for each mature tree removed.

- j. All structures for human habitation shall be located a minimum of 10 feet from the tops and toes of all slopes in excess of 10 feet in vertical height, unless otherwise approved by the City Engineer and the Department of Building and Safety.

4. Roadways/Circulation

- a. Road alignments, driveways and parking areas shall conform as closely as possible to natural topography.
- b. Existing vegetation shall be preserved to the greatest extent possible in the location of roads and shall be retained as medians or buffers wherever possible within the unimproved right-of-way.
- c. Variations in road design and construction may be permitted in order to keep grading and cut slopes to a minimum.



- d. One-way loop roads may be constructed where appropriate for the terrain.
- e. Combinations of collective private driveways, cluster parking areas and off-street parking bays are encouraged in order to minimize paved areas.

5. Fire Protection

- a. Prior to Development Review Process approval, the Fire Department shall inspect the site to identify any unique conditions that may require special fire prevention precautions, and shall communicate the findings and recommendations to the Project Review Committee.

ARCHITECTURAL DESIGN

General Guidelines

- 1. All exterior wall elevations of buildings and screen walls shall have architectural treatments that enhance the appearance of the building in conformance with the design standards specified in these guidelines. In addition, uniform materials and consistent style should be evident within a development project in all exterior elevations. Secondary materials should be used to highlight building features and provide visual interest.



2. The design of buildings and the surrounding environment should be compatible with surrounding land use and architecture, and should recognize the climate and physical setting of Southern California.
3. The architecture of buildings should reflect compatibility with surrounding character, including harmonious building style, size, color, material, and roof line. Individual dwelling units should be distinguishable from one another, and have separate entrances. Shadow patterns created by architectural elements such as overhangs, projection or recession of stories, balconies, reveals, and awnings are encouraged to contribute to a building's character and aid in climate control.

A TWO-STORY BUILDING CAN BEST RELATE TO A NEIGHBORING ONE-STORY BUILDING IF IT CONTAINS A ONE-STORY ELEMENT.



4. The mass and scale of a building should be in proper proportion to the site, open spaces, street location, and surrounding developments. (see above).
5. Multistory buildings should be detailed so as to reduce their vertical appearance as much as possible. This can be done by articulating the separate floor levels with horizontal bands, or by increasing the level of details at the street level.



6. Roof tops should be designed to be visually attractive when viewed from adjacent buildings or roadways. Roof-mounted equipment shall be concealed from public view to the extent possible.
7. Base walls and other large expanses should be light-colored. Soft tones range from white to very light pastels are encouraged. Natural colors such as off-whites, beige, and sand are also acceptable.
8. Some building elements should be brightly colored to contrast with the light colored background in order to produce a lively streetscape. Building details such as doors, chimneys, window framing, trim, railings, awnings, or light fixtures may be used to carry the accent color. However, care should be taken not to use too many bright colors or too many different types of details so as to avoid an overwhelming or cluttered-looking building.
9. Within individual residential projects, a variety of floor plans and elevations should be offered as follows:

| Number of Single Family Dwellings | * Minimum Number of Foot Prints | Minimum Number Elevations/Foot |
|--------------------------------------|---|-----------------------------------|
| 5 - 10 | 2 | 2 |
| 11 - 20 | 3 | 3 |
| 21 - 40 | 4 | 3 |
| 41 - 60 | 5 | 4 |
| 61 - 80 | 6 | 4 |
| 81 - 100 | 7 | 4 |
| Over 100 | 1 additional for each 40 du's over 100 | |

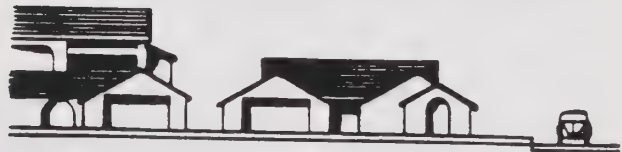
* Includes reverse footprints.

INTERIM CITY GUIDELINES



10. Appropriate and simple roof forms should be used, include shed, gable, and hip roofs, or a combination of these to achieve a varying roof line along streetscapes. Except in large industrial parks or high-rise office buildings unarticulated flat roofs should be avoided.

TO REDUCE ARCHITECTURAL MASSING, SITE THE SHORT AND LOW SIDE OF A CORNER UNIT TO THE STREET.



11. To reduce architectural massing, the short and low side of a corner residential unit should be sited toward the street (see above).
12. To create architectural congruity, where a two-story building is to be sited adjacent to a one-story structure, it should contain a one-story element.

ACCESS/CIRCULATION/PARKING

General Guidelines

1. The access and circulation of a project should be designed to provide for vehicles and pedestrians. Points of access shall comply with City access regulations and shall not conflict with other planned or existing access points. The circulation system should be planned to reduce conflicts between vehicular and pedestrian traffic by separating them to the extent possible,



minimize impacts on adjacent properties, consolidate access points where possible, and provide adequate maneuvering areas.

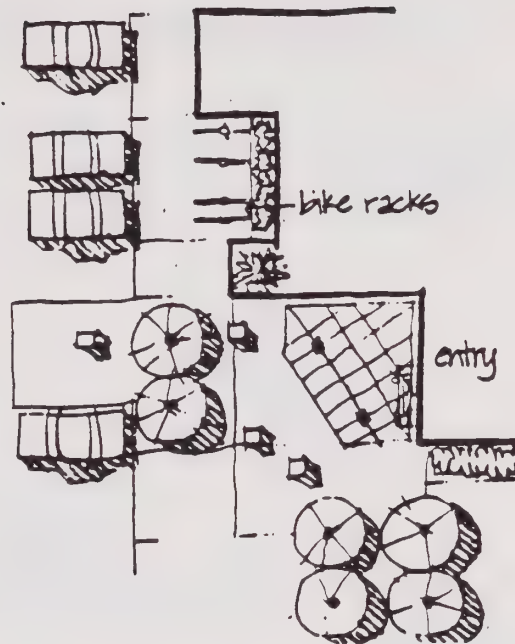


2. Parking areas should be designed to minimize visual disruption of the overall project design. Parking areas should be screened from streets through combinations of mounding, landscaping, low profile walls (not to exceed 3 feet), and/or grade separations. The design of parking areas should also minimize auto noise, glare, and increases in ambient temperature through the use of sound walls, screening with fences or landscaping, and/or the separation of parking spaces and driveways from residences. (see above).



Access/Circulation/Parking Standards

1. Pedestrian walks through open spaces and access ways to buildings shall be required in order to shorten walking distances.
2. Pedestrian walkways, corridors, plazas and other similar areas shall be highly visible and well lighted. In addition, amenities such as, but not limited to enriched paving, street furniture, raised planters should be provided to enhance the pedestrian experience.
3. Collector roadways shall minimize the use of reverse frontage walls by such treatments as increased setbacks, landscaping, berming, "side-on" cul-de-sacs, and other techniques.
4. All projects which generate vehicular traffic shall participate in the Traffic Signal Mitigation Program as adopted by the City of Moreno Valley.
5. Handicapped ramps shall be incorporated into all curb and sidewalk designs.
6. Street names within a project or neighborhood shall not incorporate the same name with minor variations such as Creekside Way, Creekside Drive, Creekside Lane, etc. All street names must be reviewed and coordinated for approval by the City Engineer.
7. Adequate guest parking shall be provided within multi-family projects. Whenever feasible, parking should be located within 200 feet of the dwellings it serves.
8. Parking lots within multiple family areas shall be limited to two double aisles of cars. The open parking areas should be clustered and treated as landscaped plazas and courts. Landscaping should be used around the perimeter of the lot, as well as within the lot. Massive paved areas are not acceptable.



9. A sufficient quantity of bicycle racks and/or lockers, as approved by the Planning Department, shall be provided at commercial centers, recreation facilities, and similar potential bicycle trip attractors. The location and design of such facilities shall be as approved by the Planning Department.
10. Within residential areas, storage of RV's, boats, and campers should be provided so as to be out of public view. Within multiple family projects, areas for the storage of RV's, boats, and campers shall be in enclosed onsite areas which are heavily landscaped to views from other developments in the community.
11. Internal roadways within commercial, industrial, and multiple family residential projects shall be designed so that direct access is available to all structures visible from a particular parking area entrance to eliminate unnecessary vehicle traffic and to improve emergency response.

INTERIM CITY GUIDELINES



12. Driveway placement shall be designed to simplify traffic flow patterns within parking areas, and the potential for conflicts. Aisles should be placed such that it is easy to reach any destination within an individual site after entering the driveway. To discourage excessive speeds within parking areas, the maximum length of straight stretches shall be designed to not exceed 400 feet, when possible. The alignment of aisles shall also accommodate pedestrian traffic by allowing patrons to walk along, rather than across, aisles to reach commercial, office, and industrial uses from their parked vehicles.
13. Figure 3 shall control the placement of driveways along public streets unless modified by an approved traffic study or approved by the City Engineer.

PUBLIC FACILITIES AND SERVICES

General Guidelines

1. All utilities shall be placed underground, or as approved by the applicable utility company and the City Engineer.
2. Unless otherwise approved by the City, public water, sewer, and drainage facilities within a project phase, as well as other backbone facilities needed to support that phase, shall be constructed prior to or concurrent with initial development within that phase.
3. To reduce the need for fire protection services, commercial and industrial buildings shall be equipped with automatic fire suppression systems in accordance with City regulations.



FIGURE
DESIGN STANDARDS FOR THE SPACING
OF DRIVEWAY ACCESSES AND STREET INTERSECTIONS

| Street Classifications | Minimum Distance from a Driveway to an Adjacent Intersection | Spacing Between Driveways |
|---|---|--|
| Specific Plan Street | Must be justified by a Traffic Study and approved by Transportation Services. Limited access only* Minimum 450'. | N/A |
| Arterial Highway 134' R.O.W. | Limited access only** <u>Minimum 450'</u> . | Limited access only, <u>Minimum 450'</u> spacing one driveway per parcel. Additional accesses must be justified by a Traffic Study (2) and approved by Transportation Services. |
| Highway Aerial (110 R.O.W.) | Limited access only** Minimum 350' | Minimum 250' spacing |
| Highway Major/ Secondary (100' R.O.W./ 88' R.O.W.) | Limited access only** Minimum 250'. Full Access Minimum 350' | Minimum 150' spacing |
| Collector Street (66' R.O.W.) | Minimum 200' *** | Minimum 100' spacing |
| Local Street | Minimum 100' *** | 100' or one driveway parcel. |

* Access limited to the minimum movements necessary to safely expedite traffic.

** Access restricted by raised medians to right turns only.

*** For residential development; as approved by Traffic Engineer.

Note: All distances shall be measured from the adjacent curb face.

Figure 3.

INTERIM CITY GUIDELINES



It is the ultimate responsibility of the project applicant to insure that facilities required to provide adequate fire flows are available. This section does not imply that every building should include a fire suppression system.

Public Facilities and Services Standards

1. Water and sewage facilities shall be installed in accordance with the requirements and specifications of the City of Moreno Valley Engineering Department, the applicable service agency, and the Riverside County Health Department. Such requirements will be applied at the subdivision, conditional or public use permit, or plot plan stage, as appropriate.
2. Drainage and flood control facilities and improvements shall be installed in accordance with requirements of the Riverside County Flood Control and Water Conservation District and the City Engineer. Such requirements will be applied at the subdivision, conditional or public use permit, or plot plan stage, as appropriate.
3. Crime prevention techniques shall be incorporated into commercial, industrial, and multiple family residential projects to reduce the need for police protection. Possible measures include, but are not limited to:
 - a. Maximizing open space and visibility within individual developments;
 - b. Locating potential crime generating uses, such as automatic tellers machines, in highly visible and well-lighted areas.
 - c. Maximizing the visibility of parking area entrances from adjacent uses and public streets.

INTERIM CITY GUIDELINES



4. To reduce the need for police protection during construction, onsite security should be provided for individual construction projects. The Police and Fire Departments shall be provided with keys to all locks on construction site gates.

LANDSCAPING AND OPEN SPACE

INTENT

One of the major components of the Interim Development Guidelines is to provide an attractive atmosphere in order to encourage businesses to locate in this area and provide a better living environment for existing and future residents. The purpose of landscaping standards is to provide the City with a greater aesthetic quality, unified architectural scene, and promote energy conservation.

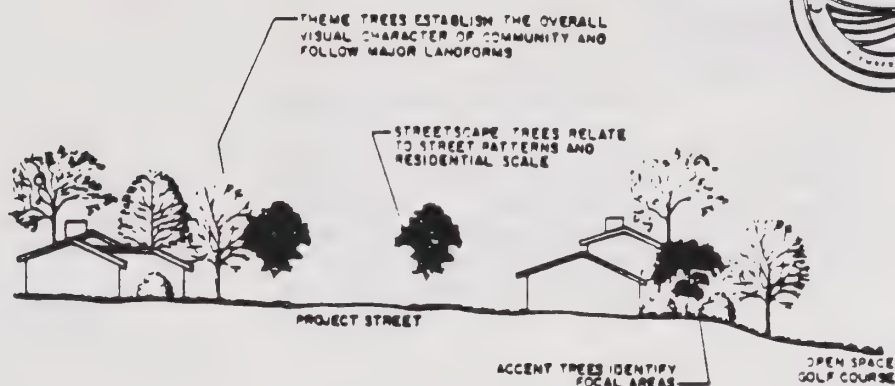
General Guidelines

1. Landscaping and open space should be designed as an integral part of project design; enhance building design, public views, and interior spaces; and should provide buffers and transitions as needed. Landscaping should provide for solar access and shade to facilitate energy conservation.
2. Landscaping shall be compatible with the local environment. Where municipal water supplies are to be used for irrigation, it shall contain low-water demand plants, and in all cases shall be capable of surviving with a minimum of maintenance.



3. Screened planting shall be utilized along the freeway edge in order to create a visual buffer.
4. The Washingtonia filifera or California fan palm trees and olive trees within a project's boundaries shall be either transplanted elsewhere within the City area, or preserved as approved by the Director of Developmental Services.
5. For projects adjacent to freeways and major and secondary highways, a landscaped area 30-feet or more in width shall be required by the Department of Developmental Services. If it is found necessary, this requirement shall also apply to projects adjacent to any street with significant features as identified by the Department of Developmental Services.
6. All proposed landscaped common green-belt, park, improved open space, parkway, and linear park areas shall include automatic irrigation systems, as well as a secondary drip system, as appropriate, for large shrubs and trees.
7. All setbacks, parkways, and non-work areas within developments under construction or graded shall be landscaped. Areas proposed for development in another phase shall be temporarily turfed, seeded, and irrigated for dust and erosions control, if said phase will not begin within six months of completions of a previous phase.
8. Required landscaping areas shall be maintained in a neat, clean, orderly, and healthful condition. This shall, at a minimum, include pruning, mowing of lawns, weeding, removal of litter, fertilization, replacement of plants when needed, and the regular watering of all vegetation.

INTERIM CITY GUIDELINES



9. All landscaped areas shall have a combination of planted trees, shrubs, vines ground cover, flowers, and or lawns which are compatible with the environment. The combinations may include rock ground cover not to exceed 20 percent of the total of any landscaped area. The selected combination of objects for landscaping shall be arranged in a harmonious manner as determined by the Department of Developmental Services. Thirty-three percent of the total number of trees to be used in the landscaping shall be of the 24 inch box size or larger and planted extensively along major streets as approved by the Director of Developmental Services. (see above).
10. Landscape design shall be coordinated with drainage plans for individual projects to maximize percolation of surface water and minimize runoff from the site. Swale designs in landscaped and grass areas may be employed to slow down runoff and maximize percolation.
11. Irrigation systems (especially in small areas such as tree wells, planters, and medians) shall be designed to prevent water from flowing off from planted areas.



Residential Standards

1. Within all residential projects, minimum 15-gallon size street trees shall be planted as follows:
 - a. Lot/unit on cul-de-sac: one tree per street frontage.
 - b. Interior lot/unit: two trees per street frontage.
 - c. Corner lot/unit: three trees per street frontage.
2. The following table set forth minimum standards for the number and size of trees for planting within multiple family projects:

| Feature | 8 -14 du/ac | 14 - 20 du/ac |
|---|-------------|---------------|
| Trees/gross acre* | 50 | 60 |
| Min. % of 24 inch box tree or larger | 15 | 20 |
| Max. % of 15-gallon trees | 10 | 10 |

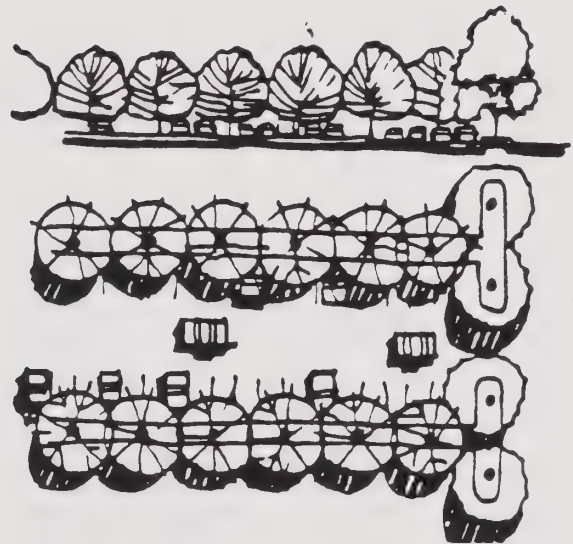
* Reduced by 20% where 10% of the trees are 36 inch box or larger.

Commercial/Industrial Standards

1. All commercial and industrial land uses shall be landscaped with a minimum of 15 percent of the net lot area devoted to landscaping. A landscape maintenance and screening plan shall be approved by the Department of Developmental Services prior to final approval.



2. Bermed landscaping shall be incorporated wherever possible within the landscaped setbacks and landscaped areas surrounding parking and loading areas. Berms shall be designed to provide interest and visual access to buildings.



3. As part of required onsite landscaping, a minimum of 10 (of which 3 to 5 percent shall be interior parking area) percent of the total offstreet parking area shall be landscaped with at least one, 15-gallon tree per three parking stalls and appropriate ground cover. The parking area shall be computed by adding the areas used for access drives, aisles, stalls, and landscaping within that portion of the premises that is devoted to vehicular parking and circulation (see above).
4. Trees shall be planted in areas of public view adjacent to structures at the equivalent of one tree per 30 linear feet of building dimensions.



5. Parking lot landscaping shall be located so as to discourage pedestrians from crossing any landscaped areas to reach building entrances and parked cars. This can be achieved through proper orientation of landscaped areas and provision of identifiable walkways.

Plant Material Standards

1. In order to achieve a uniform landscape theme, the area within the street medians, park strips, and streetscape setbacks shall have landscape materials consistent with the project's landscaping theme, as well as with surrounding landscape material.

Open Space Standards

1. Open spaces shall be integrated with pedestrian walks and defined by landscaping and other elements to create a sense of place.
2. Arbors, colonnades, and other structures that provide shade to open areas shall be utilized wherever possible.
3. The landscape design of open spaces shall be harmonious with the design of the buildings on the site and shall enhance their appearance.
4. The following shall apply to commercial and industrial uses only:
 - a. Where possible, open spaces shall be accessible to the public and shall be equipped with benches and other seating.



- b. Benches, light standards, kiosks, trash receptacles, and other street furniture in onsite open spaces shall be designed in a coordinated fashion to enhance the appearance and appeal to users of the site and open areas.

SIGNAGE

General Guidelines

1. Buildings should be designed with a precise concept for adequate signage. Provisions for sign placement, as well as sign scale in relationship to the building and readability should be considered in developing the signage concept.
2. Sign color should be compatible with building color. A light background matching the building with dark lettering is generally best visually. No more than two primary colors should be used on a sign with a third, secondary color used for accent or shadow detail.
3. In designing signs and sign messages, fewer rather than more words should be utilized to create a clean, understandable message. In addition, symbols should be used only if they are easily recognizable or represent a definable logo.
4. The overall size and shape of signs should be such that it does not detract from the message. As a general rule, letters should not appear to occupy more than 75 percent of the sign area.



5. Signs should communicate their message well, and be easily seen by people. While providing the most effective message, signs should also be highly compatible with the building and site design relative to size, color, material, and placement (see above).
6. Signs should be consistent with the proportion and scale of the building elements within the facade. For example, ground level signs should be smaller than those on higher levels and pedestrian-oriented signs should be smaller than automobile-oriented signs.



ENCOURAGED

Franklin Gothic

Souvenir Med.

Univers 65

Times Bold

Folio Ex. Bold

DISCOURAGED

MADAME

MARKIO

SPOTTYFACE

BLACKLINE

Champion

7. Hard-to-read and overly intricate type face are discouraged. The letter style which is used should be appropriate to the business and the building (see above).
8. Sign placement on a facade should complement building elements rather than block them.
9. Internal illumination of signs should be permitted on channel letters only. Signs without channel letters should be externally illuminated. Only continuous light sources shall be permitted.
10. Signs attached to the underside of a projecting canopy or protruding over a sidewalk or right-of-way may be permitted as a special type of sign if designed so as to provide pedestrian-scale atmosphere and to enhance the building front.
11. Low monument signs are encouraged to be used for major tenant identification.

INTERIM CITY GUIDELINES



12. Awning signs shall be centered on the awning to achieve symmetry, limited to the business name and logo, and sized in proportion to the awning (see above example).
13. Pole signs shall be discouraged.
14. Window signs shall be designed so as to be pleasing, aesthetically enhance shop fronts, geared to the pedestrian and be at eye level, and should be limited to less than 15 percent of the total window area.

LIGHTING INTENT

Proposed lighting plans should serve functional, safety, and aesthetic purposes. It is the intent of these guidelines to encourage innovative lighting schemes which can be easily incorporated into the overall development as an integral component.

General Guidelines

1. Adequate onsite lighting should be provided to ensure a safe environment, but not cause areas of intense light or glare.
2. Lighting fixtures should be attractively designed to complement the architecture of the project site and adjacent buildings.
3. Lighting should improve the visual identification of residences and businesses. Within commercial areas, lighting should also help create a festive atmosphere by encouraging nighttime use of areas by pedestrians.

INTERIM CITY GUIDELINES



4. Energy efficient nighttime lighting of buildings within commercial areas is encouraged. Outlining of buildings with lights creates a festive atmosphere, and encourages the use of open spaces and plazas. Do not overlight, but rather, base the lighting system on the amount of light actually required by users and viewers. As a rule of thumb, one foot candle per square foot is adequate.
5. Parking lot lighting fixtures shall not exceed 35-feet in overall height. Walkway lighting fixtures shall not exceed 20-feet in overall height.
6. Security lighting fixtures are not to project above the fascia or roof line of the building. All exterior lighting shall be shielded so as not to intrude on adjacent properties. Shields for security lighting, if provided, shall be painted to match the surface to the which the light is affixed. Security lighting fixtures are not to be substituted for parking lot or walkway fixtures, and are restricted to lighting only loading and storage locations, or other similar service areas.

SCREENING

General Guidelines

1. Fences and walls should be discouraged unless they are needed for a specific screening, safety, or sound attenuation purpose.
2. Where they are needed, fences and walls should relate to both the site being developed and surrounding developments, open spaces, streets, and pedestrian ways. The use of any fencing or



walls should be consistent with the overall design theme of the development or adjoining existing developments and should respect existing view corridors to the greatest extent feasible.

3. Walls along major boulevards shall be discouraged. Where such screening has been deemed necessary, it shall be designed so as to be integrated into the theme of the development; shall be constructed of a permanent material such as but not limited to concrete block, wrought iron, stone, and shall have landscape elements incorporated into the design to provide relief from undue heat, glare, reflection, or aesthetic inconsistencies.
4. Fences and walls should incorporate landscape elements and changes in materials, color, or texture in order to prevent graffiti; undue glare, heat or reflection; or aesthetic inconsistencies.
5. Where a wall or fence is required for security, but not for screening or sound attenuation purposes, an open, six-foot high iron fence with vertical bars spaced four inches on center is encouraged.
6. Onsite utilities and ancillary equipment should be located in inconspicuous areas or vaulted underground, away from public view. Where they are needed in public view, they should be screened with a combination of materials that best suit the overall project design theme.
7. Storage areas, loading docks and ramps, transformers, storage tanks, and other items of poor visual quality are to be screened by the use of concrete or masonry walls, dense mature landscape materials, or approved fencing material. Limitations shall be placed on the types of fences permitted.

INTERIM CITY GUIDELINES



8. All roof and ground mounted equipment shall be screened from public view on all sides within all zones.
9. All screening shall be architecturally integrated with the building design. Roof parapet walls shall be used to screen roof-mounted equipment where feasible.



GLOSSARY OF TERMS

- a. Small lot development: Single family developments where special design considerations are necessary to achieve efficient outdoor living areas. Such developments could include zero lot line, Atrium or Garden Court designs.
- b. Hillside development area: Area designated as 25% slope or above or in other areas where special standards are required which apply strictly to hillside residential areas.
- c. Gateway: Any roadway or pedestrian way which promotes a central theme of a project or marks the entrance to an area to be highlighted. Examples could include: City entrance signs, major entrances to mixed use developments, subdivision entry monuments, theme oriented roadway or walkways, hiking, biking, jogging or equestrian trail heads.
- d. Paleontological: A science dealing with the life of past geological periods as known from fossil remains.
- f. View corridors/orientation: Refers to the relationship and orientation of amenities such as parks, open spaces, major natural features (water, topo, vegetation, etc.) to the location of dwelling units, public walkways, hiking, biking, and equestrian trails along with major interior roadway alignments.
- g. Auto service bays: Any area devoted to or partially to the repair, maintenance or servicing of automobiles including but not limited to lube bays, wash racks, muffler repair lifts, painting and preparation areas, and upholstery service areas.



- h. Weed-free: All plant material not intended to be installed as new landscape materials as approved by the City.
- i. Natural vegetation: Indigenous plant materials which existed prior to the encroachment of development and can be maintained with a minimum of care.
- j. Moderate slope areas: See hillside development standards
- n. Pedestrian ways: Circulation patterns within a development exclusively used by the walking public.
- t. Human scale: The relationship between developed environment (structures, amenities, roadways, landscape, etc.) and people.



GROUPS/INDIVIDUALS CONSULTED

General Plan Advisory Committee
Development Coordination Committee
Architectural Review Committee
Planning Network
PRC Engineering
Building Industry Association
Wildan & Associates (Don Bergh, Deputy City Engineer)
Sierra Club
Audubon Society

Rod Hanway
David Leonard
Randy Hlubik
John Canty
Sandy Sandison
Lloyd Zola

INTERIM CITY GUIDELINES

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